

CITY OF ST. CHARLES

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ORDINANCE NO. 1984-M-49

AN ORDINANCE ESTABLISHING  
TITLE 12, CHAPTER 12.30  
"STREET IMPROVEMENTS" OF THE  
ST. CHARLES MUNICIPAL CODE

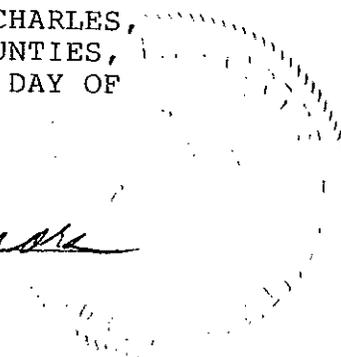
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ADOPTED BY THE  
CITY COUNCIL  
OF THE  
CITY OF ST. CHARLES  
THIS 2nd DAY OF JULY, 1984

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PUBLISHED IN PAMPHLET FORM BY  
AUTHORITY OF THE CITY COUNCIL  
OF THE CITY OF ST. CHARLES,  
KANE AND DU PAGE COUNTIES,  
ILLINOIS, THIS 13th DAY OF  
JULY, 1984

  
CITY CLERK



ORDINANCE NO. 1984-M-49

AN ORDINANCE ESTABLISHING  
TITLE 12 CHAPTER 12.30  
"STREET IMPROVEMENTS" OF THE  
ST. CHARLES MUNICIPAL CODE

REFER TO:
MINUTES <u>7-2-84</u>
PAGE <u>1998</u>

DATE OF PUBLICATION 7/16/84  
NEWSPAPER Pamphlet form

WHEREAS, the City Council of the City of St. Charles, Kane and DuPage Counties, Illinois desire to establish certain requirements for street improvements;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF ST. CHARLES, KANE AND DUPAGE COUNTIES, ILLINOIS, AS FOLLOWS:

1. That TITLE 12 of the St. Charles Municipal Code be and is hereby amended by adding CHAPTER 12.30 as follows:

**"12.30 STREET IMPROVEMENTS"**

12.30.010 **PURPOSE** The purpose of this CHAPTER is to provide standards for designing and constructing street improvements to enhance the safe passage of vehicular and pedestrian traffic; and to regulate, guide and control the development of street designations in such a manner as to manage and control traffic volumes, and the design and construction of street systems.

12.30.020 **OTHER PERMITS** Before starting any of the work regulated by this chapter, an applicant shall comply with the provisions of this Chapter and all other applicable provisions of the St. Charles Municipal Code relating to the submission and approval of preliminary and final subdivision plats, improvement plans, building and zoning permits, inspections, appeals and similar matters, and such State and Federal Statutes and Regulations as may be applicable.

12.30.030 **DEFINITIONS** For the purposes of this CHAPTER the following definitions are adopted:

- a. **CONTRACTOR.** The individual, firm, partnership or corporation contracting with the developer for the

- construction of prescribed work.
- b. **DEVELOPER.** The individual, firm, partnership or corporation planning, initiating and/or managing the street improvement which may be the owner of the land on which the improvement is being constructed.
  - c. **CURBING.** The portion of the street improvement primarily constructed to direct surface storm water to a collection point.
  - d. **MATERIALS.** Any substances specified for use in the construction of the street improvement and its appurtenances.
  - e. **MEDIAN.** The portion of a divided street separating the traveled ways for traffic in opposite directions.
  - f. **PAVEMENT STRUCTURE.** The combination of base, and surface course materials placed on a subgrade to support the traffic volume and weight for distribution to the roadbed.
  - g. **PLANS.** The plan view, profiles, cross sections, working drawings and supplemental drawings, or exact reproduction thereof, which show the location, character, dimensions and details of the work to be done.
  - h. **RIGHT-OF-WAY.** A strip of land occupied or intended to be occupied by a street, sidewalk and/or railroad, and by electric transmission lines, oil or gas pipelines, water mains, sanitary or storm sewer mains, trees, and/or for other special uses. The usage of the term "right-of-way" for land-platting purposes shall mean that every right-of-way hereafter established and shown on a final plat is to be separate and distinct from the lots or parcels adjoining such right-of-way and not included within the dimensions or areas of such lots or parcels. Rights-of-way intended for streets, crosswalks, water mains, sanitary sewers, storm drains, trees, or any other use involving maintenance by a public agency shall be dedicated to public use by the maker of the plat on which such right-of-way is established.
  - i. **ROADBED.** The graded portion of a street within side slopes, prepared as a foundation for the pavement structure and shoulders or curbing.
  - j. **ROADWAY.** The portion of the right-of-way within limits of construction.

- k. **SHOULDER.** The portion of the roadway contiguous with the traveled way for accommodation of stopped vehicles, for emergency use, and for lateral support of base and surface courses.
- l. **SIDEWALK.** That portion of the right-of-way primarily constructed for the use of pedestrians.
- m. **STORM DRAINAGE SYSTEM.** Shall include but not be limited to storm sewer piping, catch basins, manholes, inlet grates and related appurtenances and/or storm drainage ditches and related appurtenances capable of collecting and transporting a 5-year storm frequency without street flooding or damage to property.
- n. **STREET.** That portion of the roadway primarily constructed for use of vehicles.
- o. **STREET IMPROVEMENTS.** Any work related to maintaining safe pedestrian and vehicular traffic, and enhancement of the right-of-way.
- p. **SUBGRADE.** The top surface of a roadbed upon which the pavement structure and shoulders are constructed.
- q. **UTILITY.** The privately, publicly or cooperatively owned lines, facilities and systems for producing, transmitting or distributing communications, power, electricity, light, heat, gas, oil, crude products, water, steam, waste, storm water not connected with the highway drainage, and other similar commodities, including combined antenna television cables and equipment, and publicly owned fire and police signal systems and street lighting systems, which directly or indirectly serve the public or any part thereof. The term "utility" as the context shall require, shall also mean the utility company, inclusive of any wholly owned or controlled subsidiary.
- r. **WORK.** Work shall mean the furnishing of all labor, materials, equipment, and other incidentals necessary or convenient to the successful completion of the project.

12.30.040 **APPLICABILITY** This CHAPTER shall apply to all subdivisions, Planned Unit Developments and Plats of Dedication for Streets, where an engineering plan is approved after the effective date of this CHAPTER. This CHAPTER shall also apply to an unsubdivided parcel of land or to a portion or all of subdivided area where street improvements are proposed.

12.30.050 **STANDARDS FOR STREET DESIGN.**

- A. Street pavements shall be designed for a minimum 20 year

- life.
- B. In order to support street designations provided in CHAPTER 16.20.060, Table I, of the St. Charles Municipal Code the Director of Public Works may request a traffic analysis performed by a member of the Institute of Transportation Engineers and approved by him. The cost of this traffic analysis will be borne by the developer.
- C. The street and pavement structure shall be constructed in accordance with the minimum standards as set forth in CHAPTER 16.20.060, Table I of the St. Charles Municipal Code, and Standard Drawing 1 attached hereto and made a part hereof.
1. All "MINIMUM STREET WIDTH" dimensions are measured from the back of the curbing, or when curbing is not included in the project, from the edge of the driving surface.
  2. All "MINIMUM TANGENT" dimensions shall apply between reverse horizontal curves as designated in Table I, Chapter 16.20.060 of the St. Charles Municipal Code.
  3. A cul-de-sac street shall not be more than five hundred feet in length measured along its centerline from the street of origin to the end of its right-of-way, unless there are less than sixteen lots abutting the cul-de-sac street. Each cul-de-sac shall have a terminus of nearly circular shape with a minimum street diameter of 90 feet. The minimum right-of-way diameter along a cul-de-sac shall be 120 feet.
  4. Horizontal curves are required in accordance with the "MINIMUM HORIZONTAL CENTERLINE RADIUS" dimension for a deflection in the horizontal centerline at any one point in excess of 10 degrees.
- D. Provision shall be made for serving lots abutting a residential collector and major streets by either the use of marginal access street, or backing lots to the residential collector and major street with a screen planting and/or fence contained in a nonaccess reservation along the rear property line.
- E. The intersection of two streets shall form a 90 degree angle where the centerlines cross along tangent sections for each street. Where one of the street alignments is a horizontal curve, the tangent of the second street shall pass through the curve center of the intersecting street. At no time shall two curved streets intersect. The "MINIMUM TANGENT" allowed at an intersection approach shall be in accordance with CHAPTER 16.20.060,

## Table I of the St. Charles Municipal Code.

The intersection of more than two streets shall be avoided. Should specific conditions of design indicate that the intersection of more than two streets is necessary, the developer shall obtain approval from the Director of Public Works prior to proceeding with final engineering plans.

- F. Minor streets shall be aligned so that their use by through traffic will be discouraged.
- G. No half width street improvements shall be permitted.
- H. **CURBING** - Combination concrete curb and gutter, type B-6.12, or combination curb and gutter of greater widths shall be designed as part of the pavement for designated collector streets. Minor residential streets may have a concrete gutter (mountable curb and gutter). Estate area roadways, where residential lots are no less than one acre in size, may be constructed without combination curb and gutter or concrete gutters, in areas where the flow of water in the ditches will not exceed four feet per second. Any street with the exception of the ones designated "Estate" and "Minor" residential, requires concrete combination curb and gutter.
- I. The **minimum curb radius** at the intersection of two minor streets shall be twenty feet; at the intersection of a minor street and a collector street, the minimum curb radius shall be thirty feet and at the intersection of two collector streets, the minimum curb radius shall be forty feet. All street corners along designated truck routes where the intersecting street leads directly to, from or is within property zoned manufacturing purposes, shall have a minimum radius of 45 feet.
- J. **Concrete sidewalks** shall be constructed along both sides of the street and shall be located within the right-of-way one foot from the right-of-way/property lines unless a variance is granted by the City Council for a meandering sidewalk. At any time the right-of-way line is not parallel to the curbing, the sidewalk shall parallel the curbing. Their thickness and size shall conform to Chapter 12.30.060, paragraph C of the St. Charles Municipal Code.
- K. **Street signs** will be installed by the City in accordance with standards of the Illinois Department of Transportation and at all intersections. The developer shall pay for the costs, including labor and materials, for these sign installations. Any variation from the City standard signage for designating streets shall meet the approval of the Director of Public Works.

L. **Street lights** will be installed and materials furnished, including wiring, cable, poles and fixtures, by the City at all intersections and cul-de-sacs and at other locations along the streets, in accordance with Standard Drawing 2 attached hereto and made a part hereof. The developer will be required to pay for all of the costs, including labor and materials, for these street light installations. A variation from the following standards may be permitted if in accordance with generally accepted engineering practices and as approved by the City Council and the Director of Electricity. The standards for street lights shall be as follows:

1. All new streets constructed in the City of St. Charles, either by developers or by the City, will be lighted by a public street lighting system in accordance with the following illumination standards.

AVERAGE MAINTAINED HORIZONTAL ILLUMINATION

STREET CLASSIFICATION	RESIDENTIAL	INDUSTRIAL	COMMERCIAL
	<u>F O O T</u>	<u>C A N D L E S</u>	
Estate	0.2	N.A.	N.A.
Minor	0.4	N.A.	N.A.
Local	N.A.	0.6	0.8
Collector	0.6	1.0	1.0
Major	1.0	1.0	1.0
Pedestrian walkways	0.5	0.5	1.0

The average illumination shall be computed in accordance with the following formula.

$$\frac{\text{FOOT}}{\text{CANDLES}} = \frac{(\text{Lamp Lumens}) (\text{coefficient of utilization}) \text{Mtce. factor}}{(\text{spacing between luminaires in feet}) \text{ section width in ft.}}$$

2. The pole spacing used is an average value, since poles are normally installed at or near lot lines or property lines, in an effort to keep the light as close as is practical to the source of power and to avoid crossing streets and driveways with cable.
3. Materials furnished are as follows, (for a standard City residential street light installation)

American Concrete street light pole, Spec number 800 B23H6, with high rise 6 foot steel bracket with 2 inch slip fitter, with standard level poles and pole caps, 101 sky gray polished finish, or equivalent.

Luminaire, American Electric Catalog #13-222 with photo control receptacle, 175 watt mercury vapor 120/240 volt, or equivalent.

Street light underground wire to be 2 conductor number 10 A.W.G. copper UF, 600 volts.

- M. The City may require the developer to install traffic signals at intersections in accordance with design standards of the Illinois Department of Transportation. Should traffic signals not be warranted immediately, but in the future not to exceed five years after 100% build out of the platted area, or if the intersection is not part of a subdivision plat within five years following the recording of the plat of street dedication, at the election of the City, the City and developer shall enter into a signalization agreement, whereby the developer or a designated special service area would at a future date pay a portion of the cost of traffic signals. The need for traffic signals will be based upon Illinois Department of Transportation warrant standards.
- N. **Storm drainage system** shall be installed including appropriate stormwater retention and detention facilities in accordance with TITLES 16 and 18 of the St. Charles Municipal Code. The proposed storm drainage system shall discharge into an existing storm sewer or drainage way such that the existing system has adequate capacity for the additional flow and adequate provisions shall be made for surface overflow when the capacity of the storm drainage system is exceeded to ensure that buildings are not flooded or threatened by flooding.
- O. **Median and parkway areas** shall provide for drainage to the storm drainage system. Street trees shall be placed along the right-of-way in accordance with CHAPTER 12.20 of the St. Charles Municipal Code. The parkway shall be top-soiled and seeded, or sodded.
- P. **Temporary Turn Around** - For all temporary dead end streets, a temporary T-shaped or circular turn around is required at the street end. If no curb cuts for driveway access or other access are intended and if in accordance with generally accepted engineering principles no safety or maintenance problems are apparent as determined by the Director of Public Works, the Director may waive the turn around requirement. The turn around surface shall be constructed of asphalt or concrete materials.

- Q. **Land Strips.** The creation of land strips for landscaping or other purposes shall not be permitted adjacent to a proposed street right-of-way in such a manner as to deny access from adjacent property to such street right-of-way.
- R. All streets shall be properly integrated with the existing and proposed systems of thoroughfares and dedicated right-of-way as established on a City designated Official Map and/or a City adopted Comprehensive Plan.
- S. All streets shall be properly related to special traffic generators such as industries, business districts, schools, churches, and shopping centers; to population densities; and to the pattern of existing and proposed land uses.
- T. Minor or local streets shall be laid out to conform as much as possible to the topography, to discourage use by through traffic, to permit efficient drainage and utility systems, and to require the minimum number of streets necessary to provide convenient and safe access to property.

**12.30.060 STANDARDS FOR STREET CONSTRUCTION**

- A. All technical standards for street improvements shall be in accordance with the "Standard Specifications for Road and Bridge Construction", adopted October 1, 1983 by the Illinois Department of Transportation, three copies of which are and have been on file in the City Clerk's Office for at least thirty days prior to the adoption of this ordinance, which specifications are incorporated herein by reference unless otherwise superceded by this Title and sometimes referred to herein "Standard Specifications".

**B. ROADBED CONSTRUCTION**

- 1. **Right-of-way Grading** - The contractor shall grade the total right-of-way width to final grade.
- 2. **Subgrade** - Prior to placing any type of base material, the subgrade shall be inspected and approved by the Director of Public Works or his designee. Twenty-four hours advance notice shall be required for such inspections.

The use of a fabric R underliner such as "Typar" or approved equal, or other stabilization measures may be permitted by the Director of Public Works if the subgrade does not have sufficient bearing capacity as measured by the Illinois bearing ratio test. Minimum Illinois bearing ratios are set forth in Section 16.20.060, Table I, of the St. Charles Municipal Code. A fabric shall not be considered

an incremental element in design for a pavement's structural number.

3. **Base Course** - Base course materials acceptable to the City include aggregate, bituminous and concrete base courses. The aggregate base course shall be type B with a CA6 gradation. The bituminous base course shall be CA6 or CA10 gradation. For concrete streets, Section 304 of Standard Specifications shall apply.
4. **Bituminous Concrete Binder and Surface Course** - Bituminous concrete binder and surface course, shall be Class B modified or Class "I" Mixture B for the binder course, and Class I Mixture C or Class B modified for the surface course.
5. **Portland Cement Concrete Pavement** - The concrete mix shall be uniform throughout and shall contain no less than five percent (5%) nor more than seven percent (7%) entrained air by volume. Concrete in place in the form of finished pavement shall have a minimum of six percent (6%) entrained air.

The minimum twenty-eight (28) day compressive strength shall be four thousand pounds per square inch (4,000 psi). Test cylinders shall be taken and the certified compression test reports submitted to the Director of Public Works.

Curing of newly laid pavement shall be type III of Article 625.01 of Standard Specifications or as approved by the Director of Public Works.

6. **Protective Coat** - All Portland cement concrete pavements, curbing, sidewalk and other concrete construction shall receive a protective coat in accordance with the Standard Specifications.
7. **Curbing** - Concrete gutter and combination concrete curb and gutter shall be constructed in accordance with Standard Drawings 3 or 4 attached hereto and made a part hereof. Contraction joints shall be placed at ten (10) foot (min.) on centers and shall be saw cut to a minimum depth of two (2") inches from front to back of gutter and combination curb and gutter. Contraction joints shall be in place within 24 hours of concrete placement. Three quarter (3/4) inch thick premolded joint filler or

bituminous premolded joint filler for expansion joints shall be installed at 50 foot intervals. Combination curb and gutter shall be depressed at sidewalk crossings to facilitate sidewalk ramps for the handicapped in accordance with Standard Drawing 6 attached hereto and made a part hereof. Curbing materials shall be in accordance with Paragraph 12.30.060 B.5 of the St. Charles Municipal Code.

8. **Shoulder** - Shoulder materials acceptable to the City include aggregate and bituminous concrete. Aggregate should shall be type B with a CA6 gradation. Bituminous concrete shoulder shall be Class B modified. The bituminous concrete shoulder shall include a base course, in accordance with Paragraph 12.30.060 B.2 of the St. Charles Municipal Code.

C. **SIDEWALK CONSTRUCTION** - Sidewalk shall be Portland cement concrete in accordance with the material called for in paragraph 12.30.060 B.5 of the St. Charles Municipal Code, and in accordance with Standard Drawing 5 attached hereto and made a part hereof. Sidewalk shall be five (5) feet wide minimum along all collector and major streets and four (4) feet wide minimum along all minor streets and a minimum five (5) inches thick, six (6) inches thick where driveways are located if known prior to sidewalk construction. The base course shall be three (3) inches of compacted aggregate. Granular base courses shall be CA6 gradation. All sidewalks shall be provided with concrete approach ramps at all intersections, in accordance with Standard Drawing 6 attached hereto and made a part hereof.

D. **Temporary Turn Around Construction** - Temporary turn around shall be constructed in accordance with Section 12.30.060.B of the St. Charles Municipal Code.

E. **PARKWAY CONSTRUCTION** - Parkway construction shall be in accordance with Standard Drawing 5 attached hereto and made a part hereof.

1. The parkway between the sidewalk and curb shall provide cross-drainage to the street with a minimum cross slope 2% (1/4" per 1') based on a combination concrete curb and gutter, and a maximum cross slope of 25% (3" per 1').
2. The parkway shall be graded, topsoiled with four inches (4") and seeded, Class I, or sodded. When the parkway is constructed at a gradient of ten percent (10%) or greater, then sodding shall be required. Agricultural ground limestone shall not

be permitted.

#### F. DRIVE APPROACHES

1. All new drive approaches shall be Portland cement concrete or bituminous concrete, Class I or B modified respectively, in accordance with Standard Drawing 7 attached hereto and made a part hereof. Existing bituminous concrete and concrete drive approaches shall be replaced with like materials.
2. Concrete Drive Approaches - shall be six inches (6") Portland cement concrete, non-reinforced with a continuous three quarter inch (3/4") bituminous fiber expansion joint at the sidewalk and at the back of the curb. The surface shall be scored into a five foot (5') grid. This pavement shall be placed on a compacted stone or gravel base to a depth of five inch (5") minimum.
3. Bituminous Drive Approaches - shall include eight inches (8") minimum of compacted crushed gravel (CA-6) base course with two inch (2") minimum bituminous concrete surface course, Class I or Class B modified.

#### G. DISPOSAL OF EXCESS MATERIAL

1. Disposal of excess material, excluding topsoil, shall be the responsibility of the contractor. Regarding excess topsoil, the City may elect to have the contractor, with developer approval, stockpile this material for municipal use at a later date.

12.030.070 **CERTIFICATION OF DOCUMENTS** - All computations, plans and specifications prepared for compliance with this CHAPTER shall be certified by a registered professional engineer, and a copy thereof submitted with the engineering plans for the street improvement.

12.030.080 **STREET IMPROVEMENT REQUIRED INFORMATION/PROCEDURE - PRELIMINARY PLANS AND ENGINEERING PLANS.**

- A. **PRELIMINARY PLAN:** A Preliminary Plan shall have a scale no smaller than 1" = 100'. The Plan shall show curve data, including the curve radius, and curve length. The alignment shall be stationed from points of termini. The right-of-way, street and sidewalk shall be shown. A typical cross-section to scale, no larger than 1" = 10, shall also be included showing the street improvements. On such a plan, there shall also be shown the following:

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1. The location of streams, and other floodwater runoff channels.
  2. The location, if known, of drains.
  3. Storm sewers, sanitary sewer, water mains and other utilities.
  4. The location of the existing roadway and street improvements which the new street improvement shall be contiguous to or a part of.
  5. The location of any existing features including structures, trees, etc..
- B. **Engineering Plan** - The engineering plans for a proposed street improvement, shall include the following information and data:
1. Engineering plans drawn to scale on 24" X 36" format which shall include complete buildable plans for the street improvement, including the storm sewer accordance with the requirements for engineering plans specified in TITLE 16 of the St. Charles Municipal Code, prepared, signed and sealed by a registered professional engineer.
  2. Plan showing recommended locations and easements for electric, gas, communications, and telephone facilities and distribution.
  3. Street storm sewers and other storm drains to be built, the basis of their design, all in accordance with TITLE 16, of the St. Charles Municipal Code.
- C. **Procedure:** The procedure for filing the preliminary plan and engineering plans for Subdivisions and Planned Unit Developments shall be in accordance with CHAPTER 16.12 of the St. Charles Municipal Code. For all other types of proposed streets filing of the preliminary plan and engineering plans shall be done by submitting three (3) sets of documents to the City Engineer's Office with the same level of detail required under Chapter 16.12 of the St. Charles Municipal Code. The review process shall be accomplished through the Department of Public Works and presented to the Public Works Committee for recommendation for the City Council. The City Council shall be the final determining body to accept or reject the proposed street.

12.030.080 FINAL PLAT - PLATS OF DEDICATION - FEES - COLLATERAL

- A. The developer shall submit to the City a final plat for a street improvement in accordance with the requirements of TITLE 16 of the St. Charles Municipal Code for all Subdivisions and Planned Unit Developments. The developer shall submit to the City Engineer's Office a Plat of Dedication for all other types of proposed streets and in accordance with all appropriate Illinois Revised Statutes.
- B. The developer shall pay the City for the expense to the City performed in the review of the street improvement project. The schedule of fees for a street improvement shall include the following:
  - 1. Engineering review of the preliminary and final engineering plans, and construction inspection shall be based on hourly rates established by the Director of Public Works and approved by City Council in accordance with Title 16.12.150 of the St. Charles Municipal Code.
  - 2. Court reporter fees.
  - 3. Recording fees.
- C. The developer shall provide the City with collateral for the street improvement in accordance with Paragraphs 16.12.160 through 16.12.180 of the St. Charles Municipal Code.

12.030.090 VARIANCES - The City Council may grant variances from the regulatory standards of this CHAPTER, in lieu of any other variance procedure, in accordance with following requirements:

- a. **Application:** An application for a variation may be made by any person, firm or corporation, or by any office, department, board, bureau or commission of the City of St. Charles.

An application for a variance is filed with the Director of Public Works. An application for a variation shall be on a form provided by the Director of Public Works and shall contain all information required on such form including the following:

- 1. Legal description of property for which a variation is requested;

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2. The requested variation identified;
  3. The reasons which applicant relies upon as justifying the requested variation;
  4. An 8-1/2" X 11" sketch of the property showing all lot lines, street improvements, and adjoining streets.
- b. **Notice of Hearing:** No variation shall be granted by the City Council except after a public hearing before the City Council, of which there shall be a notice of the time and place of the hearing published at least once, no more than 30 days nor less than 15 days before the hearing, in one or more newspapers published in the City of St. Charles, Illinois.
- c. **Hearing:** The City Council shall hold a public hearing at which evidence in support of the variation may be presented by or on behalf of the applicant and any evidence presented by interested parties may be heard.
- d. No variance shall be granted unless the applicant for the variance can demonstrate that:
1. An exceptional economic hardship would result without the variance;
  2. The relief granted is the minimum necessary;
  3. There will be no threat to public safety or creation of a nuisance;
  4. No additional public expense will result;
  5. The property in question cannot yield a reasonable return if permitted to be used only under the condition allowed by the regulations governing the district in which it is located;
  6. The plight of the owner is due to unique circumstances, or;
  7. The variation, if granted, will not alter the essential character of the locality.

12.030.091 **RESPONSIBILITY** - The administration of this CHAPTER shall be the responsibility of the Director of Public Works.

12.030.092 **INTERPRETATION** - In the interpretation and application of this CHAPTER, the provisions expressed herein shall be held to be the minimum requirements and shall be

liberally construed in favor of the City of St. Charles and shall not be deemed a limitation or repeal of powers granted by State Statutes.

**12.030.093 ABROGATION AND GREATER RESTRICTIONS** - It is not intended by this CHAPTER to repeal, abrogate or impair any existing easements, covenants or deed restriction. However, where this CHAPTER imposes greater restrictions, the provisions of this CHAPTER shall prevail.

**12.030.094 DISCLAIMER OF LIABILITY** - This CHAPTER shall not create liability on the part of the City of St. Charles or any officer or employee thereof for any damages which may result from reliance on this CHAPTER or on any administrative decision made thereunder.

**12.030.095 PENALTY** - Any person who violates any provision of this CHAPTER or fails to comply with any of its requirements shall upon conviction thereof be fined not more than \$500 (Five Hundred Dollars). Each day such violation continues shall be considered a separate offense.

**12.030.096 CORRECTIVE ACTIONS** - Nothing herein contained shall prevent the City of St. Charles from taking such other action as it deems necessary to prevent or remedy a violation of this CHAPTER".

2. That after the adoption and approval hereof the Ordinance shall (i) be printed or published in book or pamphlet form, published by the authority of the Council, or (ii) within thirty (30) days after the adoption and approval hereof, be published in a newspaper published in and with a general circulation within the City of St. Charles.

6/25/84

**PRESENTED** to the City Council of the City of St. Charles, Illinois, this second day of                      July           , 1984.

**PASSED** by the City Council of the City of St. Charles, Illinois, this second day of                      July           , 1984.

**APPROVED** by the Mayor of the City of St. Charles, Illinois, this second day of                      July           , 1984.

  
MAYOR

ATTEST:

  
CITY CLERK

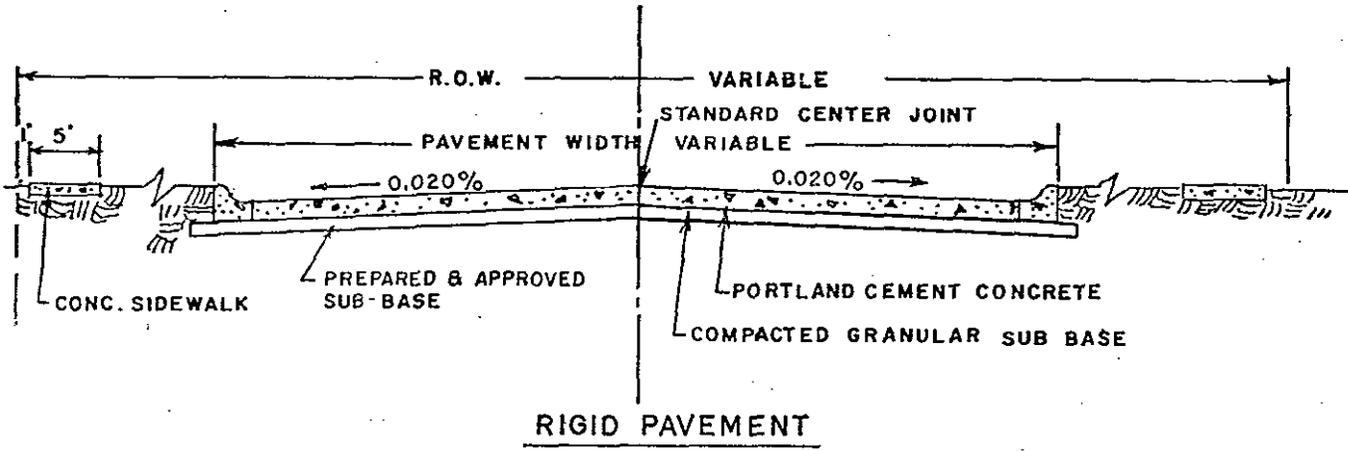
COUNCIL VOTE:

Ayes:     10    

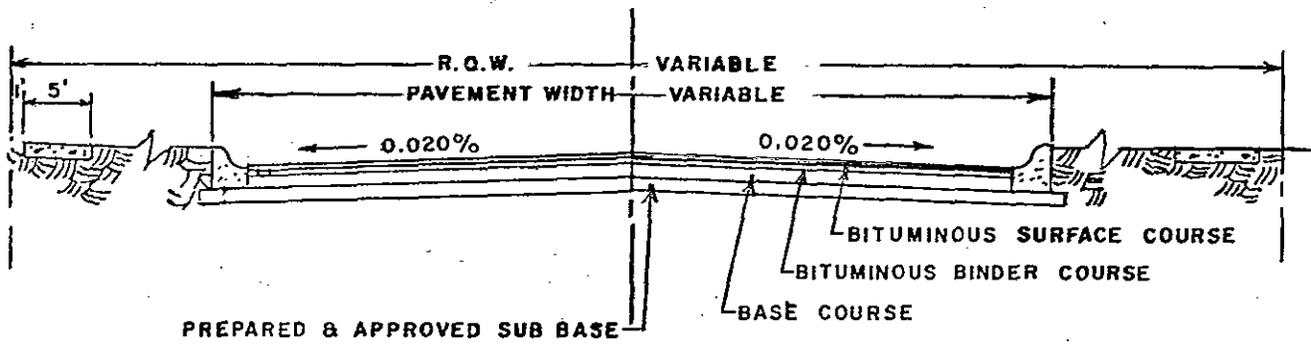
Nays:     0    

Absent:     0

# PAVEMENT CROSS SECTION (URBAN)



RIGID PAVEMENT

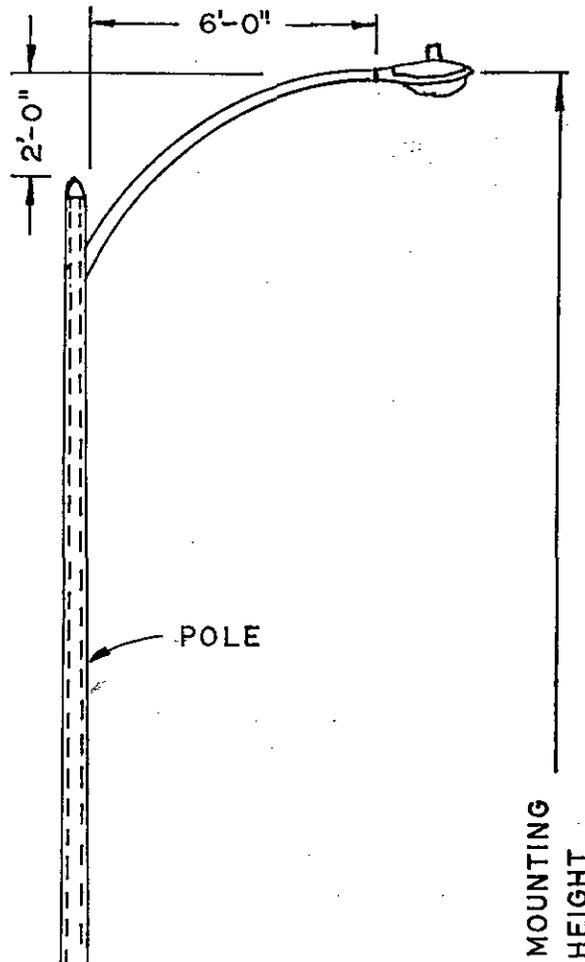


FLEXIBLE PAVEMENT

NOTES:

1. SOIL ANALYSIS AND TRAFFIC COUNTS MUST BE USED FOR DETERMINING TYPICAL SECTION.
2. CURBING SHALL BE IN ACCORDANCE WITH THE ST. CHARLES MUNICIPAL CODE.

# STREET LIGHT DETAIL RESIDENTIAL STREET



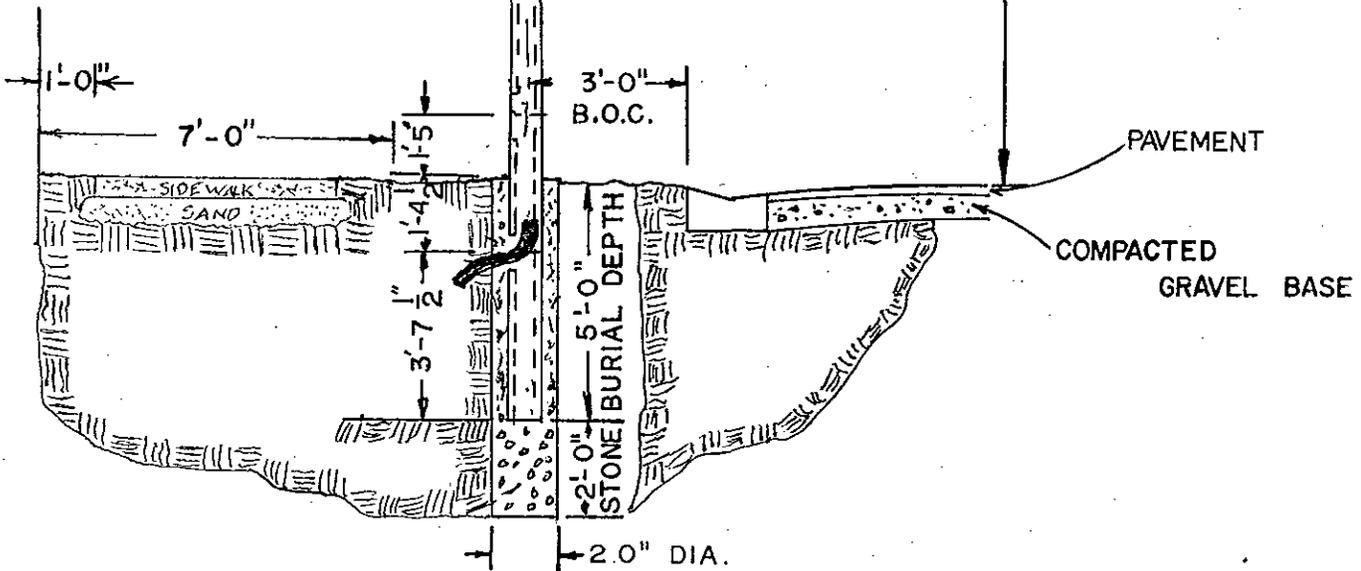
NOTES:

- 1) CABLE OPENING IN BASE  
9 1/2" x 3"
- 2) HAND HOLE OPENING IN  
POLE, 10 1/2" x 5"
- 3) CABLE OPENING IN TOP  
OF POLE, 2 1/2" x 1 5/8"
- 4) 9" GRAVEL, TO BE  
COMPACTED.
- 5) 1" WASHED STONE FOR  
BASE

POLE

MOUNTING  
HEIGHT

PROPERTY LINE



PAVEMENT

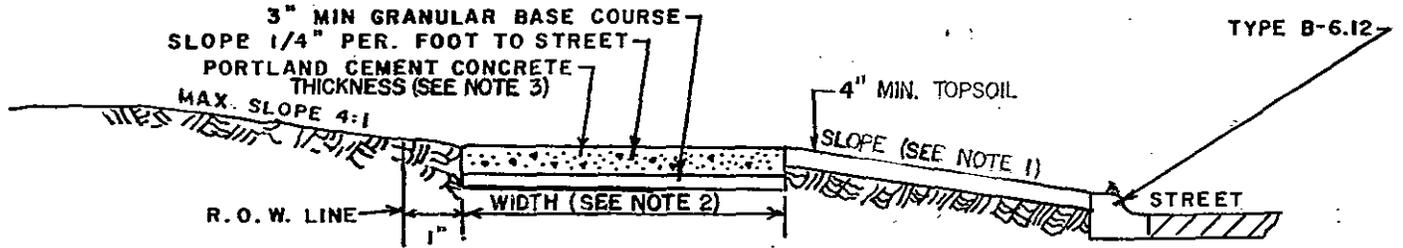
COMPACTED  
GRAVEL BASE

2.0" DIA.

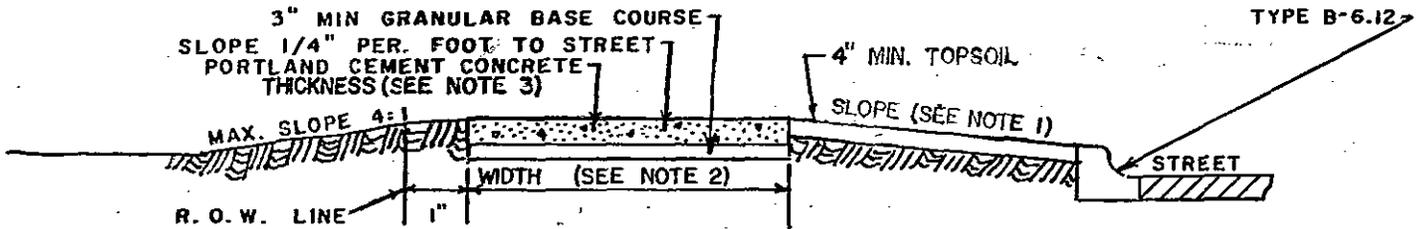




# SIDEWALK & PARKWAY RESTORATION



TYPICAL SIDEWALK SECTION IN CUT AREA

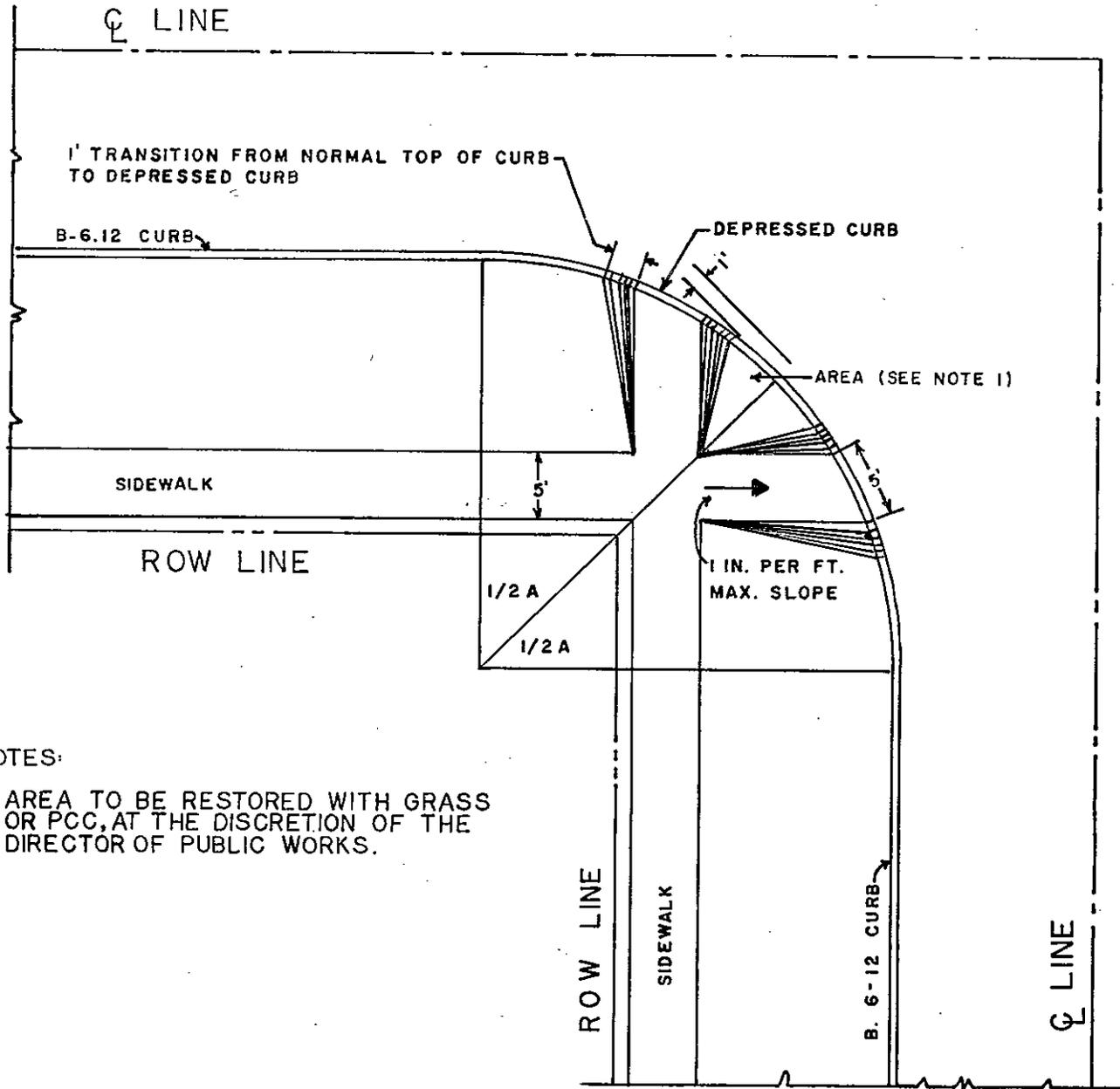


TYPICAL SIDEWALK SECTION IN FILLED AREA

**NOTES:**

- 1) SLOPE SHALL BE AT MINIMUM 1/4" / FT, HOWEVER NOT TO EXCEED 4:1 (HORIZ. : VERTICAL)
- 2) SIDEWALK WIDTH: ALONG COLLECTOR AND MAJOR STREETS 5 FEET, ALONG MINOR STREETS 4 FEET.
- 3) SIDEWALK SHALL BE A MINIMUM OF 5" THICK, AND A MINIMUM OF 6" THICK AT DRIVEWAYS.

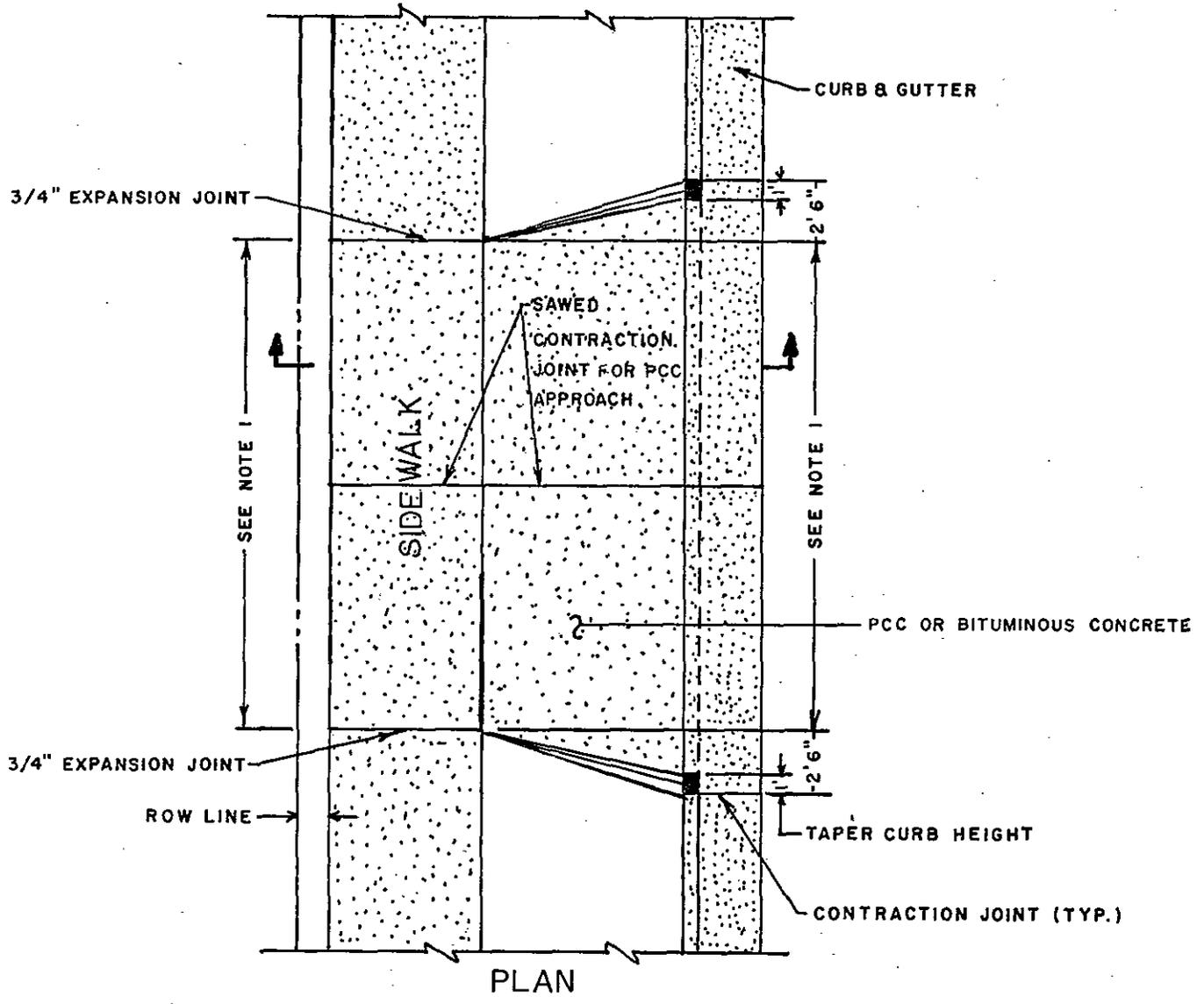
# TYPICAL SECTION SIDEWALK RAMPS



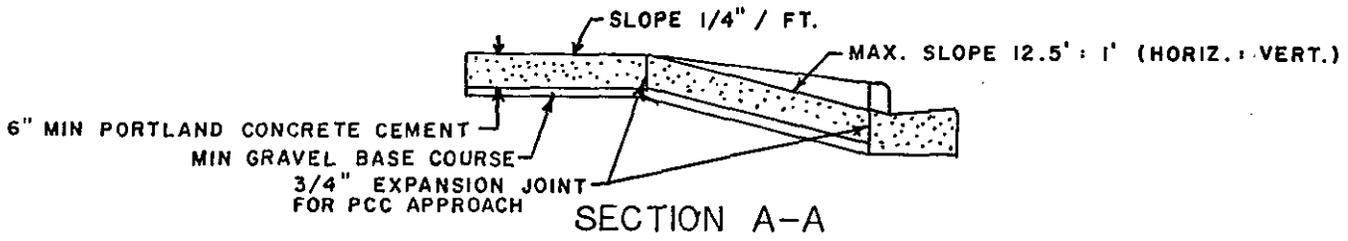
NOTES:

- 1. AREA TO BE RESTORED WITH GRASS OR PCC, AT THE DISCRETION OF THE DIRECTOR OF PUBLIC WORKS.

# DRIVEWAY APPROACHES



PLAN



SECTION A-A

NOTE:-

1. DRIVEWAY WIDTHS:  
RESIDENTIAL - 10' MIN, 24" MAX
2. DRIVEWAY SHALL BE AS NEAR 90° DEGREES, AS SITE CONDITIONS PERMIT BUT NOT LESS THAN 60° DEGREES.
3. THE CONCRETE GUTTER THRU THE DRIVEWAY APPROACH SHALL BE IN ACCORDANCE WITH CITY STANDARD DRAWING NO. 3.

