

17.24.070 – Design of off-street parking facilities

All off-street parking facilities shall comply with the following standards:

A. Setbacks and Yard Coverage

1. Single-family, two-family and townhouse dwellings

Off-street parking facilities and access drives may be located in any yard, but shall not cover more than the front or exterior side yard in which it is located than is specified below:

- a. For circular driveways, up to fifty percent (50%) of the front yard, if both access points intersect with the front lot line, b) up to fifty percent (50%) of the exterior side yard, if both access points intersect with the exterior side lot line or c) up to twenty-five percent (25%) of the total area of the front and exterior side yards, if one access intersects the front lot line and the other intersects the exterior side lot line.
- b. For all other driveways, forty percent (40%), except that one driveway of at least 18 feet wide generally perpendicular to the street is permitted regardless of the percentage of the yard it occupies.

(Ord. 2014-Z-14 § 1)

2. All uses other than single-family, two-family and townhouse dwellings

- a. Off-street parking facilities shall not be located within the front or exterior side yard applicable to parking facilities as required in the district regulations; where no specific yard requirement for parking facilities is specified in the district regulations, the requirement applicable to buildings in the district shall apply.
- b. An access drive generally perpendicular to a public street may traverse any front or exterior side yard but shall not cover more than twenty-five percent (25%) of the front or exterior side yard in which it is located, except that one driveway of at least 24 feet wide generally perpendicular to the street is permitted, regardless of the percentage of the yard it occupies.

3. Other requirements for all uses

- a. Off-street parking shall not be located in a Landscape Buffer (Section 17.26.070).
- b. Buildings, parking decks, carports and other structures containing off-street parking shall comply with the front, rear and side yard setback requirements for such structures applicable to the district in which the lot is located.
- c. When any parking lot containing five or more open off-street parking spaces abuts a lot in any RE, RS, RT or RM district, it shall be set back a minimum of five (5) feet from the lot line, and the setback area shall be landscaped in accordance with Chapter 17.26, "Landscaping and Screening".

(Ord. 2013-Z-16 § 2; Ord. 2006-Z-12 § 3; Ord. 1991-Z-7 § 1; Ord. 1960-16.)

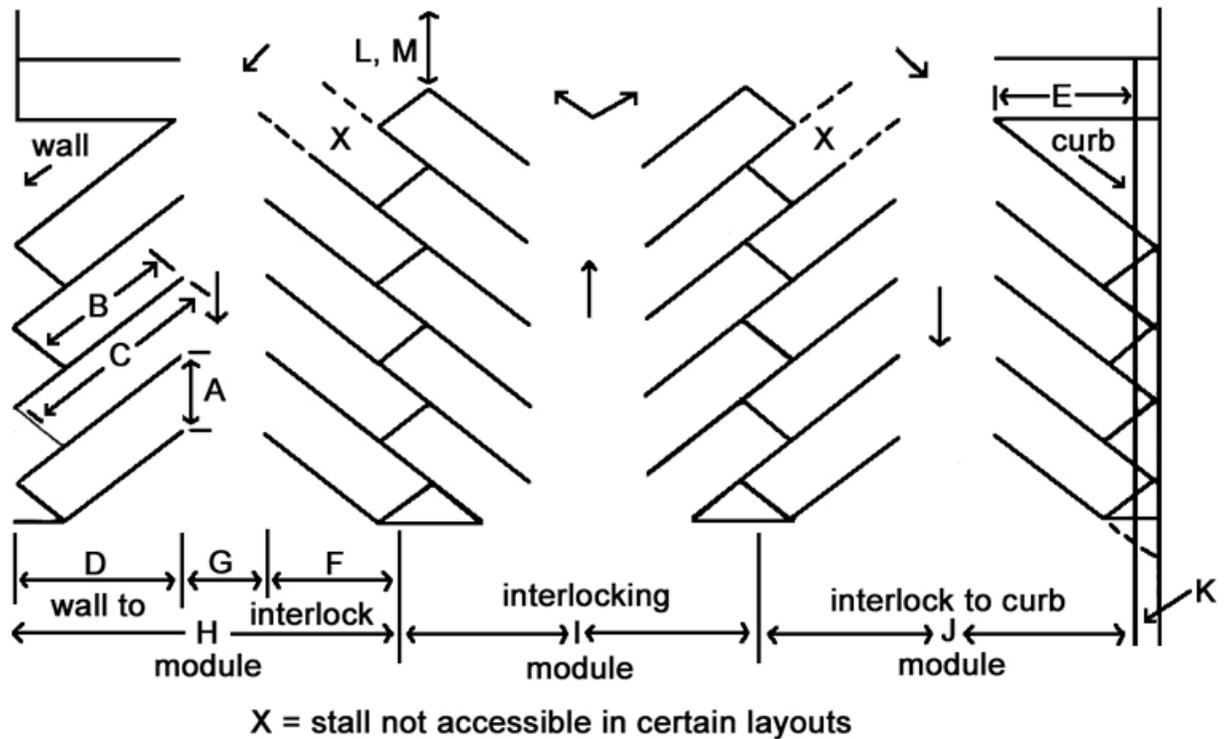
B. Dimensions

Off-street parking spaces shall be at least nine (9) feet in width, except required off-street parking spaces in surface parking lots accessory to grocery stores and multiple-family dwellings shall be at least nine and one-half (9½) feet in width. Off-street parking spaces, aisles and driveways shall have a minimum vertical clearance of seven (7) feet. The minimum dimensions of off-street parking spaces, aisles and layout shall be in accordance with the following standards in Table 17.24-1 and Figure 17.24-1:

TABLE 17.24-1 DIMENSIONS OF OFF-STREET PARKING SPACES (IN FEET)					
9 FOOT SPACE:	DIAGRAM:	45°	60°	75°	90°
Stall width parallel to aisle	A	12.7	10.4	9.3	9.0
Stall length	B	18.0	18.0	18.0	18.0
Stall line length	C	27.0	23.2	20.4	18.0
Stall depth to wall	D	19.1	20.0	19.7	18.0
Stall depth to face of curb	E	17.7	18.3	17.8	16.0

<p align="center">TABLE 17.24-1 DIMENSIONS OF OFF-STREET PARKING SPACES (IN FEET)</p>					
Stall depth to interlock	F	15.9	17.8	18.6	18.0
Aisle width	G	12.0	15.0	21.5	24.0*
Module, wall to interlock	H	47.0	52.8	59.8	60.0
Module, interlocking	I	43.8	50.6	58.7	60.0
Module, curb face to interlock	J	45.6	51.1	57.9	58.0
Bumper overhang	K	1.4	1.7	1.0	2.0
One-way cross aisle	L	14.0	14.0	14.0	14.0
Two-way cross aisle	M	24.0	24.0	24.0	24.0
<p>*In the CBD-1 District, aisle width for 90° layouts may be reduced to 23 feet within a parking garage/structure where the spaces are available to the general public and/or customers.</p>					
<p>*In the CBD-1 District, aisle width for 90° layouts may be reduced to 22 feet within a parking garage/structure where the spaces are assigned to owners or tenants or the parking is otherwise not available to the general public.</p>					
Parking parallel to aisle:	8-foot width, 22-foot length				

Figure 17.24-1



C. Access

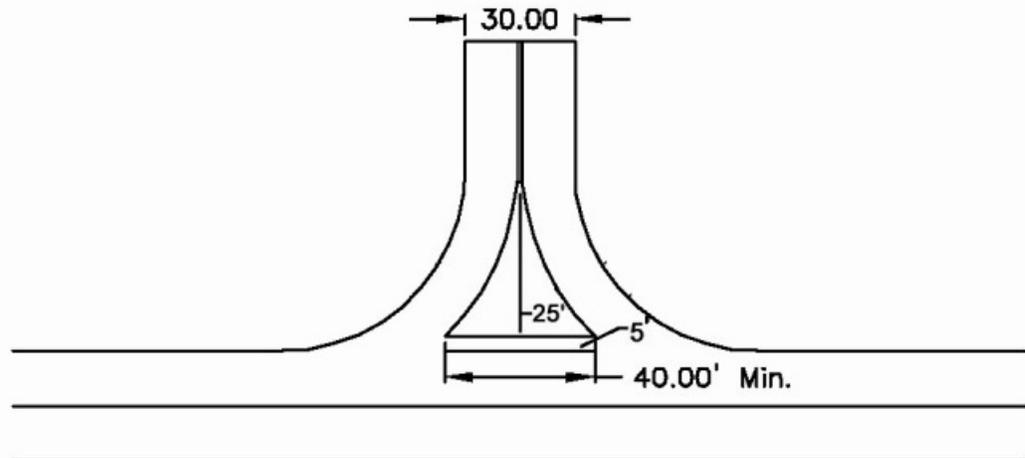
1. Adequate access shall be provided for all off-street parking spaces by means of driveways conforming to the dimensions set forth in this Title. Off-street parking lots shall not be designed so as to necessitate backing movements for ingress or egress, except for entering or exiting individual parking spaces. Vehicular ingress and egress shall be from minor streets wherever possible, rather than from arterial or collector streets.
2. One-way driveways shall be clearly marked with appropriate signs or pavement markings. If traffic conditions in the vicinity of the site warrant the restriction of turning movements or access to or from a parking facility, signs and driveway modifications necessary to accomplish said restrictions shall be provided by the property owner. The Director of PublicWorks may impose such restrictions based upon applicable State and Federal requirements or, if no requirements are applicable, good engineering practice.
3. Driveway width adjoining public streets, measured parallel to the curb or edge of pavement at the property line, shall be as follows:
 - a. For single and two-family dwellings, a maximum of twenty-four (24) feet in width for a single driveway, or for a lot with two driveways, a maximum of eighteen (18) feet in width per driveway. No more than two driveways shall be permitted per lot. (Ord. 2013-Z-16 § 3.)
 - b. Two-way driveways shall be limited to a maximum of thirty (30) feet in width, except for high traffic generators, which shall be limited to a maximum of forty-eight (48) feet in width. High traffic generators shall be considered those land uses which typically generate over 100 trips during their peak hour, as established by the Institute of Traffic Engineers (ITE), or that generate over 750 trips in an average day.
 - c. Driveways forty-eight (48) feet or more in width shall contain medians of a minimum width of four (4) feet and a minimum length of twenty-five (25) feet and shall be offset from the right-of-way five (5) feet. Driveway medians for driveways that are less than forty-eight (48) feet in width require design approval from the Director of Public Works.
 - d. In the M-2 district, driveways designed to be utilized by semi-trucks to access designated loading docks may be expanded to a maximum of forty-eight (48) feet in width when adjoining non-arterial and non-collector streets, and no driveway median shall be required. (Ord. 2009-Z-20 § 2.)
4. Driveways that are designed to prohibit left turns in and out shall have a channelizing island, as shown in Figure 17.24-2 (Channelizing Island), with the exception of driveways entering streets with barrier

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medians. Limited turn driveways are subject to the following requirements:

- Channelizing island width, measured parallel to the street: Forty (40) feet minimum.
- Channelizing island length: Twenty-five (25) feet.
- Channelizing island offset from right of way: Five (5) feet.
- Driveway width: Maximum Thirty (30) feet.

Figure 17.24-2: Channelizing Island



**TO PREVENT LEFT-TURN
INGRESS/EGRESS MOVEMENTS**

- Newly established or expanded driveways shall be located outside of any public street intersection sight distance triangle, as determined by the standards of the American Association of State Highway and Transportation Officials (AASHTO), to the extent practical as to allow access to a lot, subject to a determination by the Director of Public Works that the proposed driveway design follows good engineering practice and will not impede public safety.

(2009-Z-20 § 3.)

D. Slope

No area of any parking facility shall have a slope of more than five percent (5%). In general, no access ramp shall have a slope of more than eight percent (8%).

E. Curbing and Bumper Stops

Bumper stops, wheel stops or curbing shall be provided to prevent vehicles from damaging or encroaching upon any sidewalk, landscaped area or parking lot island, fence, wall or building. Curbing or other methods shall be in provided to protect landscaping in accordance with Chapter 17.26.

F. Striping

Parking spaces shall be delineated with paint or other permanent materials, which shall be maintained in clearly visible condition.

(Ord. 2008-Z-24 § 16.)

G. Surfacing

All parking facilities, loading facilities, Outdoor Motor Vehicle Display, and outdoor storage of commercial vehicles, recreational vehicles, trailers and intermodal containers shall be graded and paved with bituminous concrete, portland cement concrete, concrete pavers, brick pavers, or pervious pavement material. All pavement for parking areas shall conform to the following, or have an equivalent structural number:

- Parking spaces and Outdoor Motor Vehicle Display spaces:

- 1 ½ inches Class I bituminous surface course
- 1 ½ inches Class I binder Course
- 8 inches Class I base course (compacted CA-6 or approved equal) or
- 6 inch Portland cement concrete
- 4 inches compacted CA-6 or approved equal

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2. Minor access drives (i.e., parking access aisles):
 - 1 ½ inches Class I bituminous surface course
 - 1 ½ inches Class I binder course
 - 10 inches Class I base course (compacted CA-6 or approved equal) or
 - 6 inch Portland cement concrete
 - 4 inches compacted CA-6 or approved equal
3. Main access drives, truck access drives and loading areas:
 - 1 ½ inches Class I bituminous surface course
 - 2 ½ inches Class I binder course
 - 10 inches Class I base course (compacted CA-6 or approved equal) or
 - 9 inch Portland cement concrete
 - 4 inches compacted CA-6 or approved equal
(Ord. 1999-Z-21 § 1.)

H. Drainage

Open off-street parking facilities shall comply with the drainage requirements of Title 18 of the St. Charles Municipal Code, as amended

I. Visibility

Parking facilities, sidewalks and landscaping shall be located so that visibility at interior and street intersections is not inhibited. Landscaping and other obstructions at such intersections shall comply with the sight triangle requirements of Chapter 17.22.

J. Lighting

Lighting shall be provided for parking lots with five or more parking spaces in accordance with Chapter 17.22.

K. Landscaping and Screening

All parking lots shall be landscaped in accordance with Chapter 17.26, Landscaping and Screening.

(2002-Z-2 : § 1; 1998-Z-15 : § 1; 1993-Z-5 : § 1; 1991-Z-7 : § 1)